

Manual Addendum For Golden EagleTM

The following pages attached in this addendum should be used instead of the manual when referring to components on Golden EagleTM.

NOTE

The method used for starting the engine will vary slightly depending on previous conditions of the machine. Regardless of which of the following conditions exist, the ignition switch must be depressed to restart the engine.

DRY START: (Applies when the machine is first placed in service; when the machine has been allowed to run completely out of fuel; or, when the machine has not been in recent use). When starting from the dry condition (all fuel drained, consumed or evaporated from the carburetor), additional actuation of the priming pump will be required to move fuel from the tank to the primer bulb.

COLD START: (Applies after the engine has been started initially; has been stopped before running out of fuel; and has been allowed to cool). Under these conditions, very little actuation of the priming pump is required, as some fuel will usually remain in the carburetor. One actuation of the priming pump is usually sufficient under these conditions.

HOT START: (Applies after the engine has been started initially; has been stopped before running out of fuel, and has not been allowed to cool before restarting). Under these conditions, actuating the priming pump is not usually required as some fuel will usually remain in the antechamber.

FLOODED START: (Applies after too much fuel has reached the antechamber by excessive operation of the priming pump). See section on STARTING A FLOODED ENGINE.

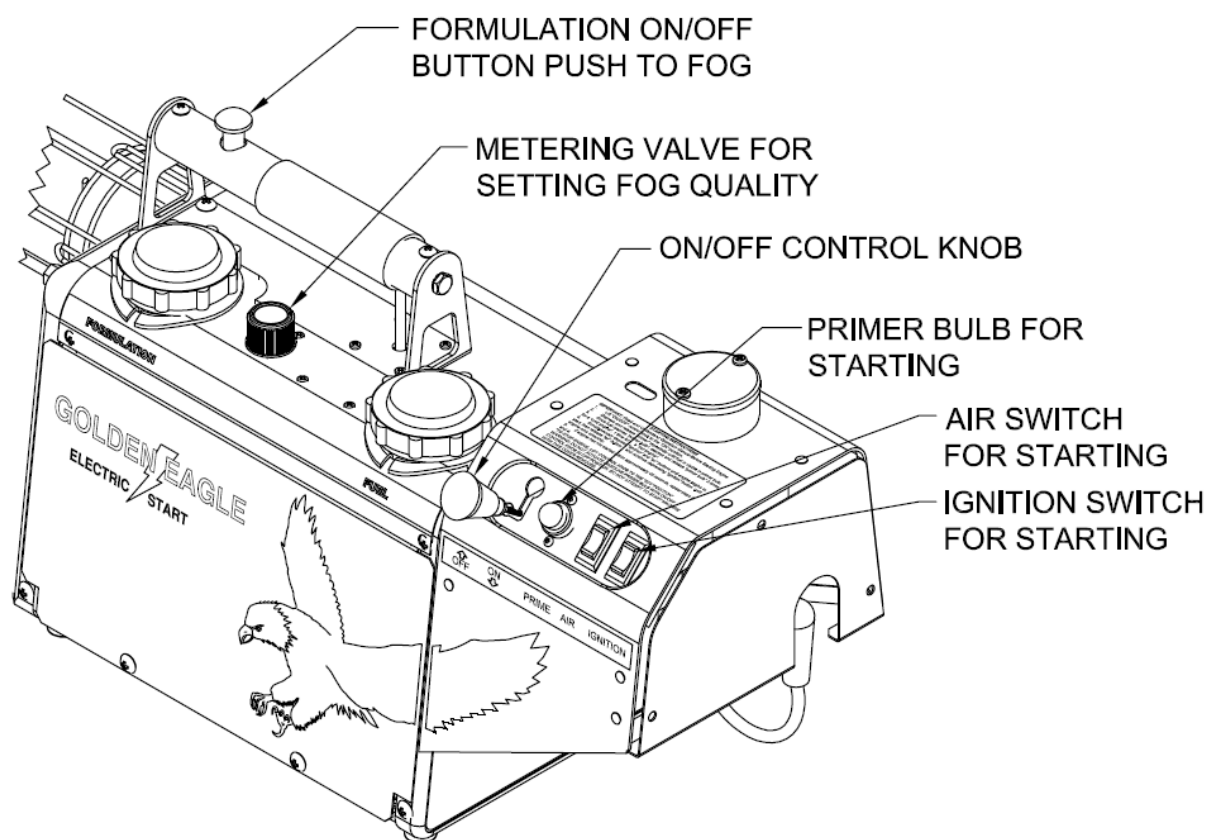


FIGURE 1

STARTING THE ENGINE

1. Verify that the FORMULATION ON/OFF button is released and that the FORMULATION METERING VALVE has been rotated fully clockwise (CW) until the Stop is reached.

2. Lift the ON/OFF Control and place it in the "ON" position.

WARNING

DO NOT LEAVE THE MACHINE UNATTENDED WITH THE ON/OFF CONTROL IN THE ON POSITION, ESPECIALLY IF THE MACHINE HAS BEEN RUNNING. IF THE ENGINE IS HOT AND THE ON-OFF CONTROL IS IN THE ON POSITION, THE MACHINE MAY SELF START.

3. Press and hold the Ignition Switch and listen for the audible buzzing or clicking sound of the Electronic Ignition firing the Spark Plug.

4. Depress and release the "Primer Bulb" repeatedly until the fuel is visible in the Bulb. Once fuel reaches the Bulb, depress and release the Bulb (3) times for a "cold engine", (1) time for restarting a "hot engine". (See Page 8 for starting conditions.)

CAUTION

Excessively depressing the primer bulb will flood the engine.

WARNING

FLOODING MAY RESULT IN A LOUD EXPLOSIVE SOUND AND CAUSE SOME SMALL AMOUNT OF FLAME TO BE EMITTED FROM THE DISCHARGE (EXHAUST) END OF THE ENGINE.

CAUTION

If flame emits from the discharge (exhaust) end of the engine tube, the engine is flooded. STOP. See the section STARTING A FLOODED ENGINE.

5. Simultaneously press and hold the Ignition and Air Switches until the Engine begins to start.

6. Once the Engine starts, release the Air Switch and continue depressing the Ignition Switch, until the Engine runs smoothly.

If the Engine has not started within approximately 40 seconds, repeat steps 4, 5, 6.

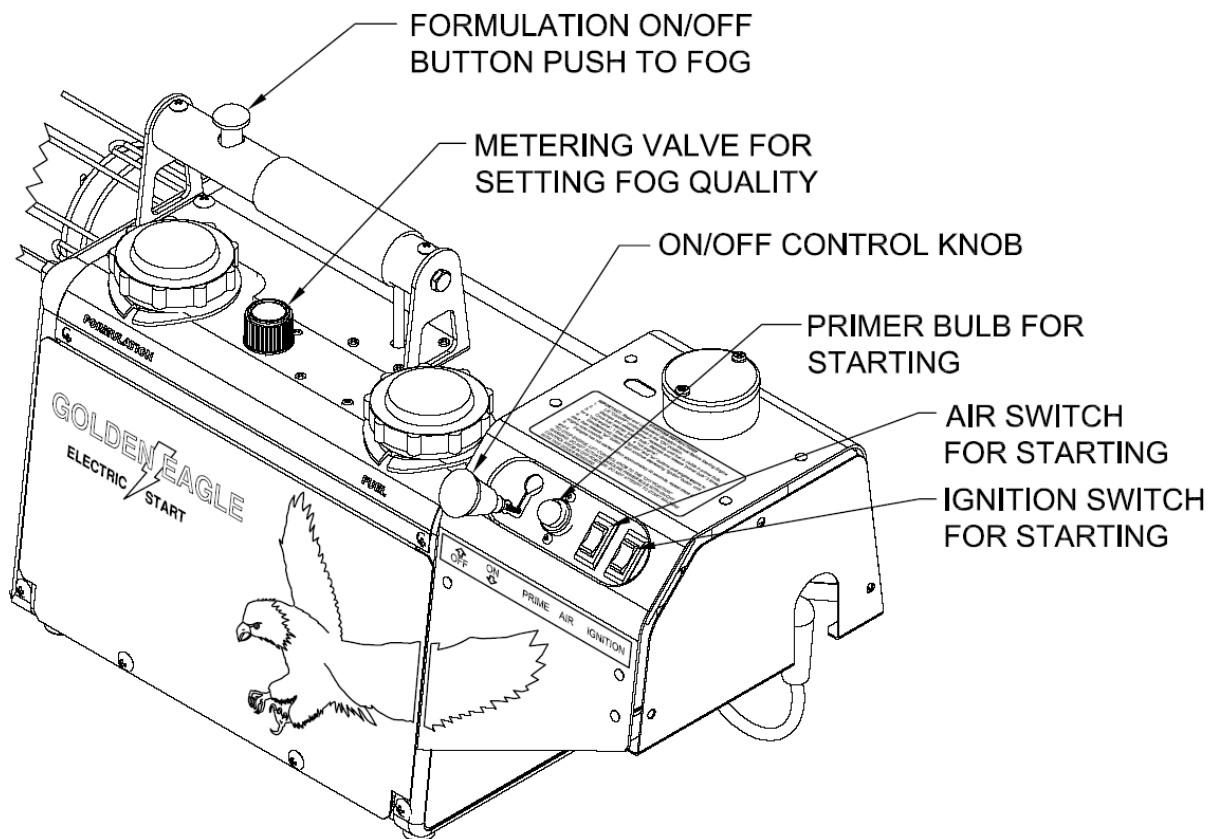
If the Engine does not start, and the sound of the Electronic Ignition is not heard, release the IGNITION SWITCH and proceed to the TROUBLE SHOOTING section of this manual.

WARNING

DO NOT ACTUATE GASOLINE PRIMER BULB WITH ON/OFF CONTROL KNOB IN OFF POSITION. DO NOT ATTEMPT TO START MACHINE WITH THE CARBURETOR COVER REMOVED.

NOTE

"Flooded" means that the fuel-air mixture around the spark plug has become too rich (too much fuel/or the amount of air available) to ignite.



5. After setting the fog quality, the fog may be started and stopped by pressing and releasing the FORMULATION ON-OFF BUTTON.

STOP FOGGING

1. When fogging is complete, release the FORMULATION ON-OFF BUTTON and rotate the FORMULATION METERING VALVE clockwise (CW) until the stop is reached.
2. Stop the engine by lifting the ON-OFF CONTROL and placing it in the OFF position.

CLEAN UP

1. Drain any unused Formulation from the tank into its original container for proper storage.

CAUTION

Store all formulations where they are not accessible to children or other persons who *may* not be aware of potential dangers involved.

Do not store formulations in unmarked or otherwise improper containers.

Do not store formulations in empty food or beverage containers or in any container marked for another substance.

Do not re-use empty formulation containers for other purposes. Dispose of empty formulation containers in accordance with the formulation label instructions.

2. After properly storing the formulation, perform the "AFTER EACH USE" maintenance operations under the MAINTENANCE section of this manual.

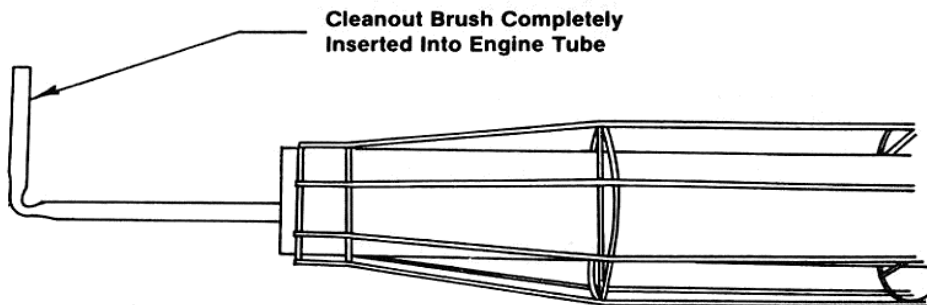


FIGURE 3

2. Continue rotating the tool in the same direction and pull back and remove the brush from the tube.

NOTE

It is not normally necessary to push and pull hard on the brush handle; however, if the engine tube is not cleaned regularly, it becomes increasingly difficult to clean. Push and pull gently and continue rotating.

Loose carbon will be removed from the tube when the engine is next started.

AFTER EVERY EIGHT (8) HOURS OPERATION

Clean the Formulation Filter

1. Remove the FORMULATION FILTER and clean it with detergent and water.

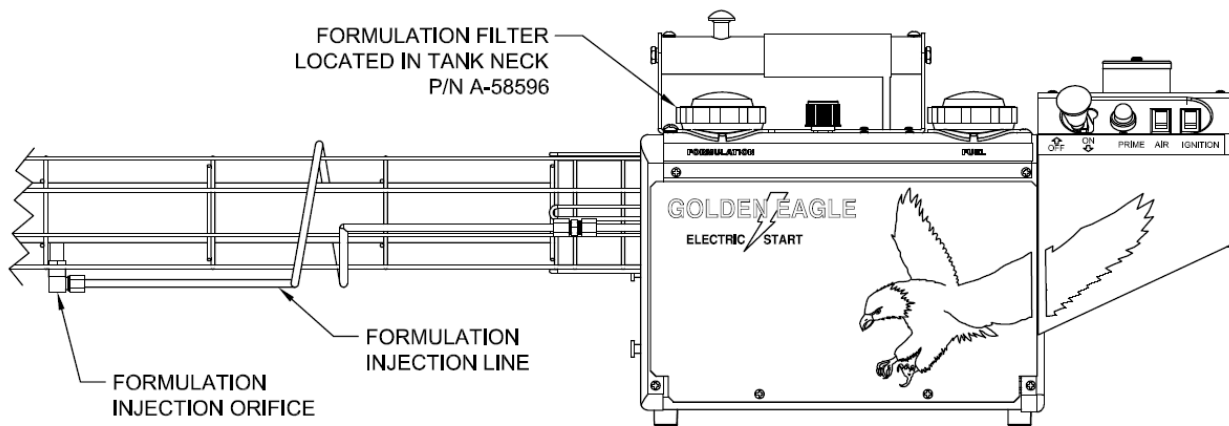


FIGURE 4

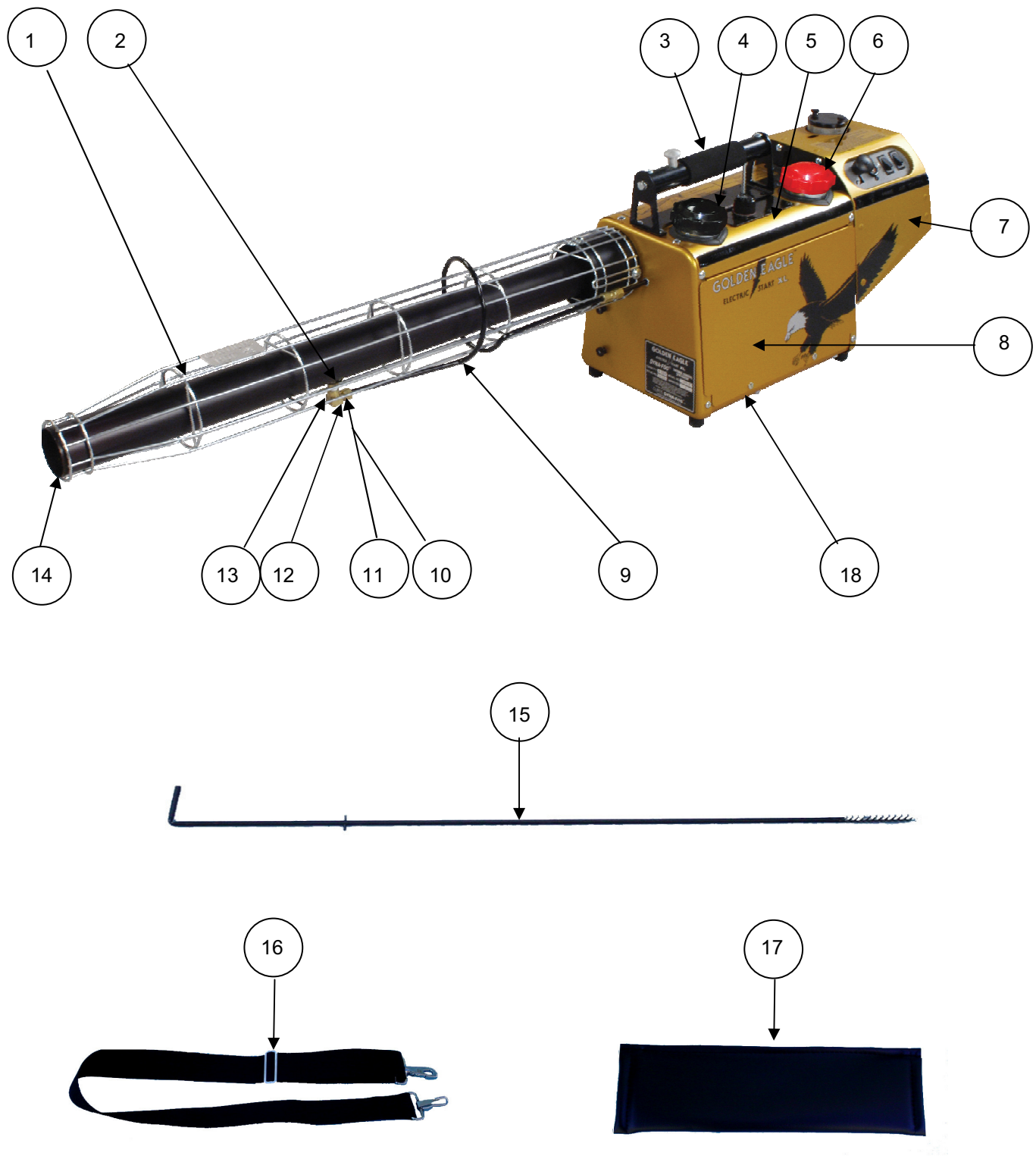


FIGURE 10

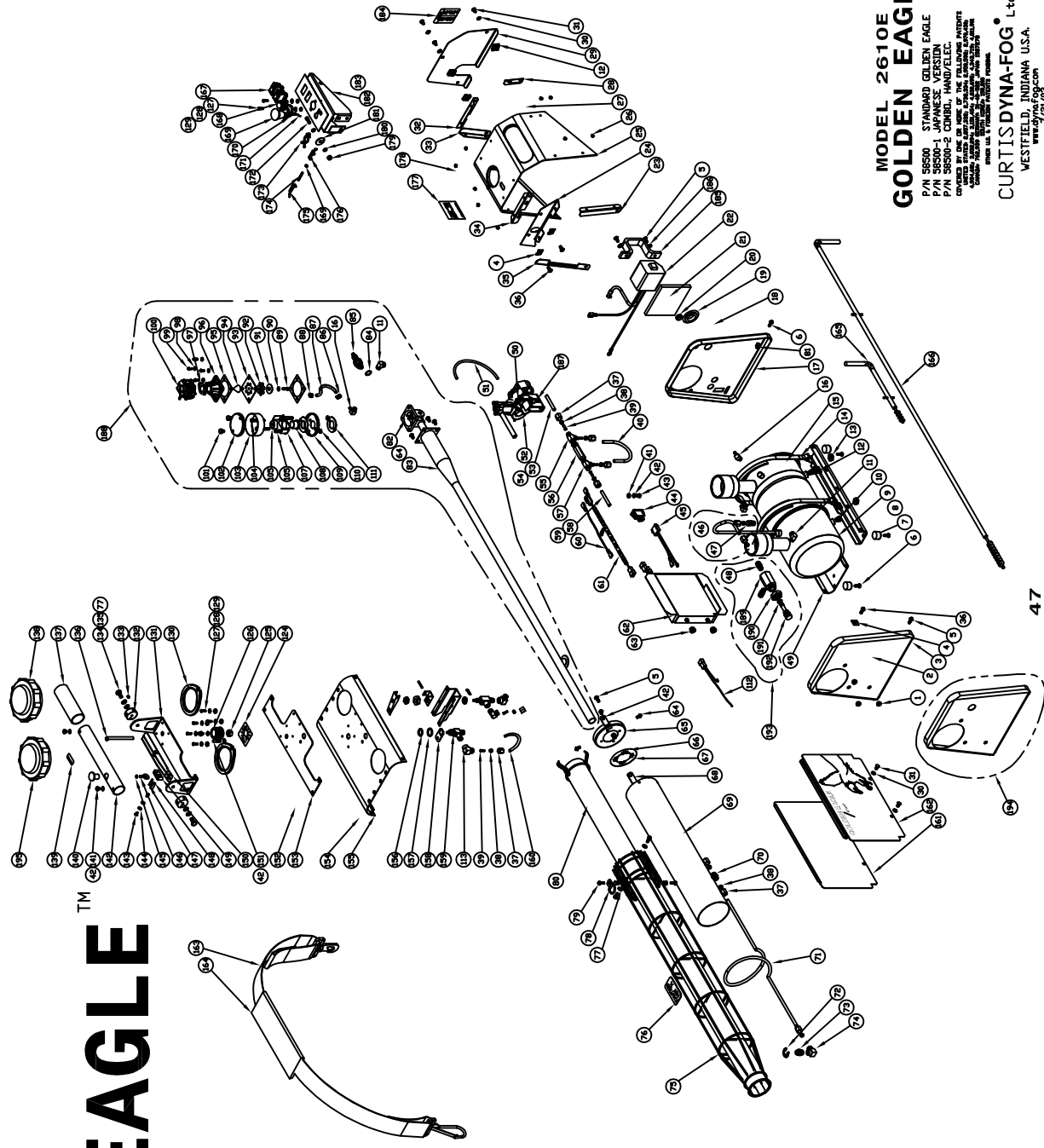
TANK-SIDE VIEW OF MACHINE

REF. NO.	CURTIS PART NO.	DESCRIPTION
FIGURE 16		
1	58522	Formulation Tank Ay.
	C58522	Formulation Tank
	A10105	Standpipe Connector
	A58523	Formulation Standpipe
	A85748	Elbow, 1/8 MPT -1/4T
	G195463	Nut, 1/4 Tube
	G114628	Sleeve, 1/4 Tube
	A21120	Union Elbow
	A21081-1	Plug, 1/8 NPT-Male
2	N58565	Line Ay., Formulation Tank/Check Valve
	A58212-3	Tube
	A58239	Insert, Brass 1/4 Tube
	G145463	Nut, 1/4 Tube
	G114628	Sleeve, 1/4 Tube
	A743123-3	Spring, Anticrimp
3	N58564	Line Ay., Formulation Tank/Metering Valve
	A58212-4	Tube
	A58239	Insert, Brass 1/4 Tube
	G145463	Nut, 1/4 Tube
	G114628	Sleeve, 1/4 Tube
4	C58254	Fuel Tank and Barb Ay.
5	N58608-1	Fuel Line Ay.
	58713-2	Tubing, Tygon, .250, 5.38"
	80296-12	Clamp, hose, D25, Double Wire
	58316	Filter, 3/16 X 60 Microns
	58713-3	Tubing, Tygon, .250 OD
6	N58605	Line Ay., Check Valve/Tee
	A58212-1	Tube
	A58239	Insert, Brass 1/4 Tube
	G145463	Nut, 1/4 Tube
	G114628	Sleeve, 1/4 Tube
7	A58609	Switch, N.O., Rocker
8	N58615	Screw, 6-32 x 1/2 CRPH
9	A58534	Primer Blub Ay.
	A63428	Primer Blub
	A58711-1	Tube, Vinyl (7.0" long)
	A58711-2	Tube, Vinyl (6.5" long)
10	B58272-4	Control Knob Ay.

DYNA-FOG[®]
GOLDEN EAGLE[™]

[illegible]

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MODEL 2610E
GOLDEN EAGLE[®]

P/N 58500 STANDARD GOLDEN EAGLE
P/N 58500-1 JAPANESE VERSION
P/N 58500-2 COMBO, HAND/ELEC.
COVERED BY ONE OR MORE OF THE FOLLOWING PATENTS:
UNITED STATES 4,677,280; 4,736,334; 4,839,020; 4,679,480;
4,334,840; 4,647,349; 4,315,404; 4,488,484; 4,343,774; 4,811,014;
4,394,400; 4,624,699; 4,699,025; 3,942,352; JAPAN 5819778

CURTIS DYNA-FOG[®] Ltd.
WESTFIELD, INDIANA U.S.A.
www.dynafog.com
7/21/03

50500-1 JAPANESE GOLDEN EAGLE USES ITEM # 193 & 194. DISREGARD ITEM # 3

50000-2 GOLDEN EAGLE WITH UNIVERSAL BATTERY HARNESS USES ITEM # 112. DISREGARD
ITEM #'S 62 & 63.

NOT SHOWN 58237 CARBURATOR REPAIR KIT
58238 CARB CASSETT/MANUACRM KIT